

This is a true copy of the Development Plan  
Approved by Maribyrnong City Council on  
the 15 June 2004, under Schedule 4 to the  
Development Plan Overlay of the  
Maribyrnong Planning Scheme

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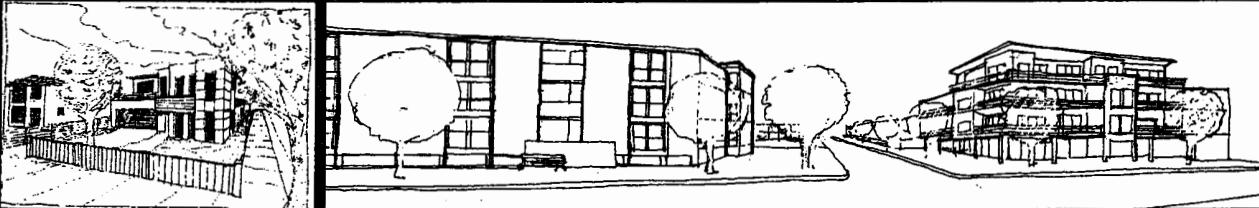


David Walmsley  
Manager – Urban Planning  
16 June 2004

## **PART 1:**

### **DEVELOPMENT PLAN**

**Rosamond Road, Birdwood Street  
and Mephan Street, Maribyrnong**



**Prepared by Tract Consultants Pty Ltd**

In Association with:

Factotum Pty Ltd

Reference: PART1\_303066\_09

March 2004

# CONTENTS

1	Introduction – Part 1 : Development Plan	3
2	Background	4
3	Description of the Site	6
4	Design Response	9
5	Development Plan Requirements	11
6	Conclusion	17

# INTRODUCTION

This document comprises two parts. Part 1 is the Development Plan for 54 lots, known as "Allara" but will be referred to as 'the Site' throughout these documents. The Urban Design Guidelines form part Part 2 of the document.

## 1.1 Rationale for the Development Plan

The development objective for Allara is to establish a high quality, sustainable residential precinct which develops over time in a coordinated manner, making the most efficient use of the land for conventional to medium density housing.

The site seeks to encourage the contemporary or "modern Australian style". This is demonstrated by the contemporary designs "pioneered" by some quality medium density housing in Melbourne. At present there is a clear movement in housing towards clean lines, and a modern Australian style.

## 1.2 Planning Scheme Requirements

The Site was rezoned in July 2002 by Amendment C9 to the Maribyrnong Planning Scheme to a Residential 1 Zone with a Development Plan Overlay (Schedule 4) and an Environmental Audit Overlay. It is zoned for residential purposes in accordance with the Residential 1 Zone provisions and is affected by the Development Plan Overlay Schedule 4 and an Environmental Audit Overlay.

This plan has been prepared in accordance with the Development Plan Overlay (Schedule 4) of the Maribyrnong Planning Scheme which requires that a Development Plan be prepared to coordinate the development of the site for residential use prior to a permit being granted to subdivide or develop the land. This Schedule is attached as **Appendix A**.

Any permit granted for use, subdivision or construction of buildings and works must be generally in accordance with this Development Plan.

## 1.3 Agreements

The S.173 Agreements for design construction and on-going maintenance for open space and road verges have been completed to Councils satisfaction. An agreement relating to the construction of an acoustic fence along the eastern boundary of the site has also been agreed to and will be registered on the title of affected future land owners.

# BACKGROUND

2

## 2.1 Melbourne 2030

In October 2002, the State Government launched Melbourne 2030, a new Metropolitan Strategy aimed at reducing urban sprawl and increasing housing opportunities and densities within Melbourne.

The Allara site is well placed to take advantage of the Implementation Plans of Melbourne 2030, being located close to existing public transport and Highpoint Shopping Centre, which has been nominated as a Principal Activity Centre.

One of the intentions of Melbourne 2030 is, through the implementation of policies to continue "to protect the liveability of the established areas and to increasingly concentrate major change in strategic redevelopment sites such as activity centres and underdeveloped land. While a good supply of land for development will be maintained in growth areas, over time there will be a shift away from growth on the fringe of the city."

The Allara site, in its recent rezoning to Residential 1, represents one of these strategic development sites. The Design Guidelines and Development Plan that has been prepared to guide future development of the site fully recognises the strategic importance of the site in relation to Maribyrnong and wider Melbourne.

### 2.1.1 Key Direction 1 - A More Compact City

This Development Plan fully supports statements made under Direction 1 - A More Compact City, including:

*"Melbourne 2030 will support and encourage development opportunities in line with the proposed household distributions for new households..... in order to:*

- *encourage a greater proportion of new dwellings at strategic redevelopment sites (particularly Principal Activity Centres and Major Activity Centres) within established metropolitan urban areas, to reduce pressure for urban expansion"*

The Design Guidelines and Development Plan proposes a variety of medium density housing and in relation to Lot 15, home office / mixed use opportunities. The intention of development is to deliver diversity and vitality to activate the site. One of the most important considerations for the site is to reintegrate land that has been a separate industrial entity since its inception into the surrounding development and the wider Maribyrnong community.



Valnere Street - Indicative housing styles in the area north of the site are predominately post-war, Californian Bungalows

# BACKGROUND

2

## 2.3.2 Key Direction 3 - A Greener City

Direction 3 - A Greener City of Melbourne 2030 states:

*"Melbourne 2030 supports and builds on such strategies while recognising the need to manage our urban systems in a way that minimises impacts on the environment. It will widen the focus from individual sites to ensure awareness of cumulative impacts on local, regional and global ecosystems, with action occurring across a number of interrelated areas.*

*The Strategy includes actions that will help reduce greenhouse gas emissions and promote measures to improve air quality. Efforts to recycle waste water for non-potable uses will be encouraged, water harvesting areas protected from incompatible uses, and development and catchment areas will be planned and managed more comprehensively. Melbourne 2030 will also work to reduce the negative impact of stormwater on waterways and bays."*

The Allara development will implement this Key Direction through the use of environmental innovations to promote a more ecologically sustainable development. In particular, the development proposes the implementation of environmental practices and infrastructure that will provide energy efficiency from a number of perspectives, including, amongst other things:

- > Proximity of the site to existing public transport infrastructure, including trams and buses.
- > Reduction in reliance on energy for heating and cooling through environmental design of new buildings.
- > Household water saving and wastewater reduction measures and cost effective recycling processes;
- > Stormwater capture and / or quality improvements, processes and practices; and
- > Waste reduction strategies including construction, operation and demolition strategies.



Indicative housing in Merlyn Street. Note predominantly post-war construction, varied fencing styles, large blocks and low scale forms.

# DESCRIPTION OF THE SITE

3

## 3.1 Location

The site is located west of the Maribyrnong River, approximately 1 km south of the Highpoint Shopping Centre on Rosamond Road, Maribyrnong. It is immediately north of the existing Footscray Primary School which is located on the corner of Rosamond Road and Mephan Street. Maribyrnong Secondary College and VUT are within close proximity to the site. The site is approximately 7 kilometres from the Melbourne CBD. Please refer to **Figure 1 –Location Plan**.

## 3.2 Site Analysis

The site area comprises 2.7 hectares of land with three street frontages of approximately 74m to Rosamond Road, 121m to Birdwood Street and 223m to Mephan Street. The land is an "L" shaped parcel with existing industrial uses abutting the land to the east, as well as in the north west corner to Rosamond Road and Birdwood Street. A Site Context Plan has been prepared and identifies the existing conditions of the site and context of surrounding uses. (**Figure 2**)

The development area is relatively flat and previously comprised a disused brick and corrugated iron factory/warehouse as well as a separate corrugated iron structure. The main building had an approximately 8m high brick wall extending for 160m along the Mephan Street frontage. It was a dominant building in the streetscape which created substantial shadowing of the street and public realm.

All three roads accessing the site have been constructed to urban standards with two way vehicular movements provided on each. Each of these roads also comprise kerb and channel footpaths, and nature strips with a variety of tree species. This provides a quality streetscape environment, and a consistency through the estate.

## 3.3 Neighbourhood Description

Surrounding uses and development include:

- Land known as the Vickers Systems Pty Ltd office/warehouse which is located on the corner of Rosamond Road and Birdwood Street. This abuts the Allara development area north-west;
- Land known as the R-Max factory/warehouse which abuts the Allara development area to the east. This land is used for the manufacture of plastics and polystyrene products;
- The Footscray North Primary School and a nursing home/day hospital opposite the site on Mephan Street;
- A group of factoriettes and a Big W warehouse to the south-east on Mephan Street;
- The Rosamond Road Special School at the corner of Rosamond Road and Inkerman Street to the south-west; and,
- Residential development on the north side of Birdwood Street and on the west side of Rosamond Road.

Although the subject site is situated in an eclectic neighbourhood of light industrial, and community buildings on Mephan street, the residential area north provides the dominant character and homogenous urban fabric.

The site's immediate context (including the recent residential subdivision of land east of R-Max) is illustrative of the transitional nature of the area with aged industrial sites being redeveloped for residential purposes in this location.

# DESCRIPTION OF THE SITE

3

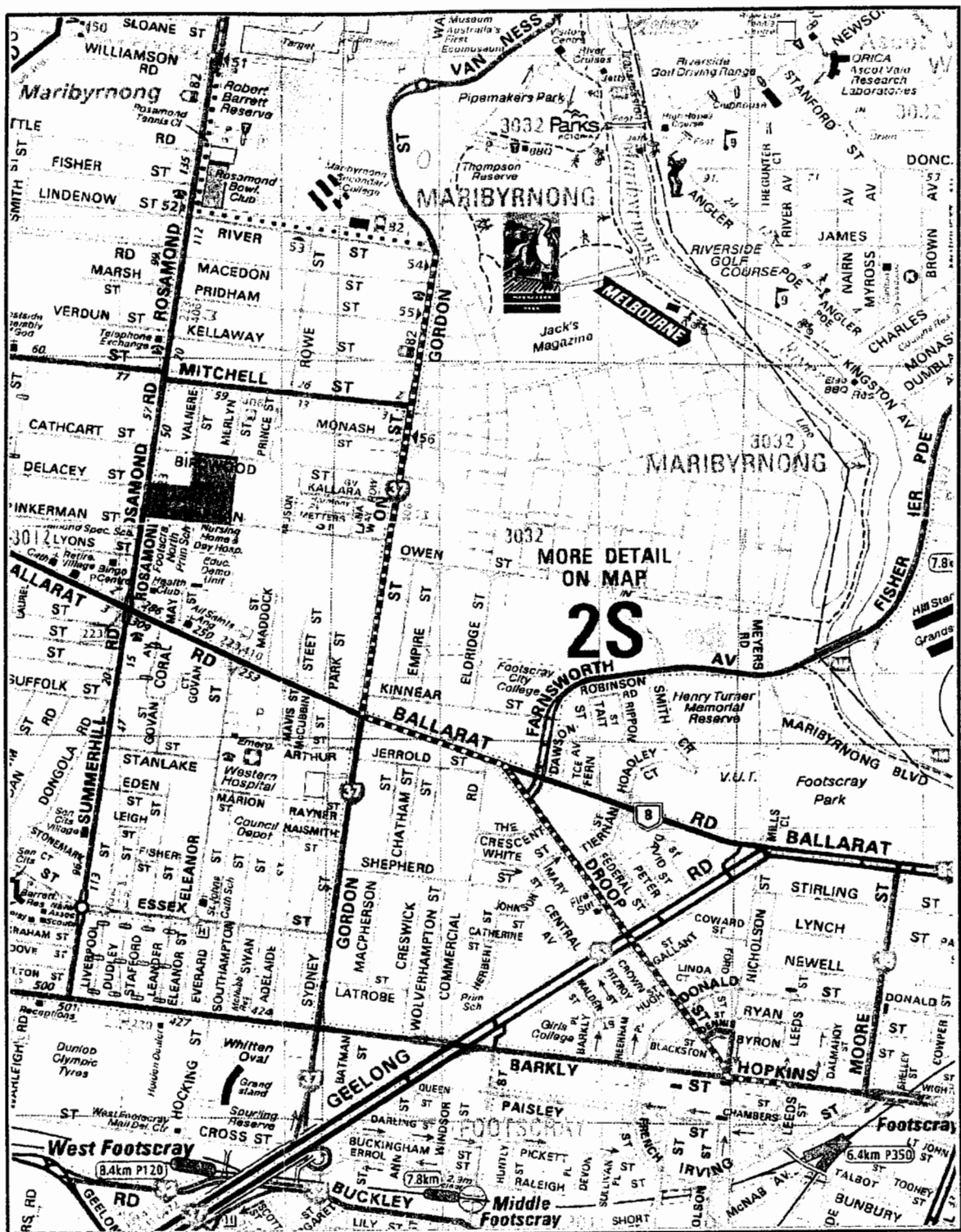


Figure 1 - Locality Plan

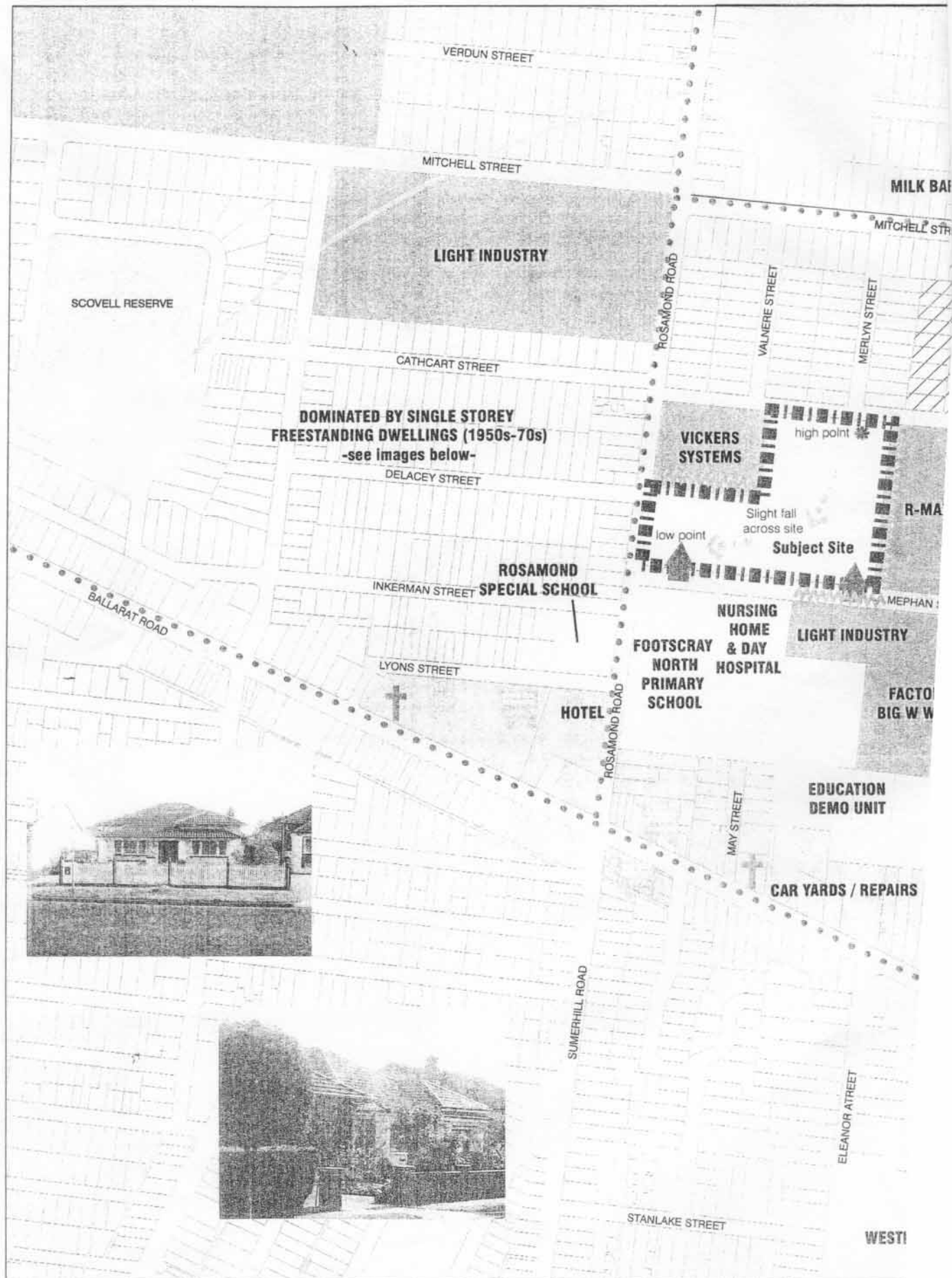


VUT Student Village (1.5km)  
Medway Golf Club (2km)

Highpoint Shopping Centre (1.5km)  
Maribyrnong Defence Site (2.5km)

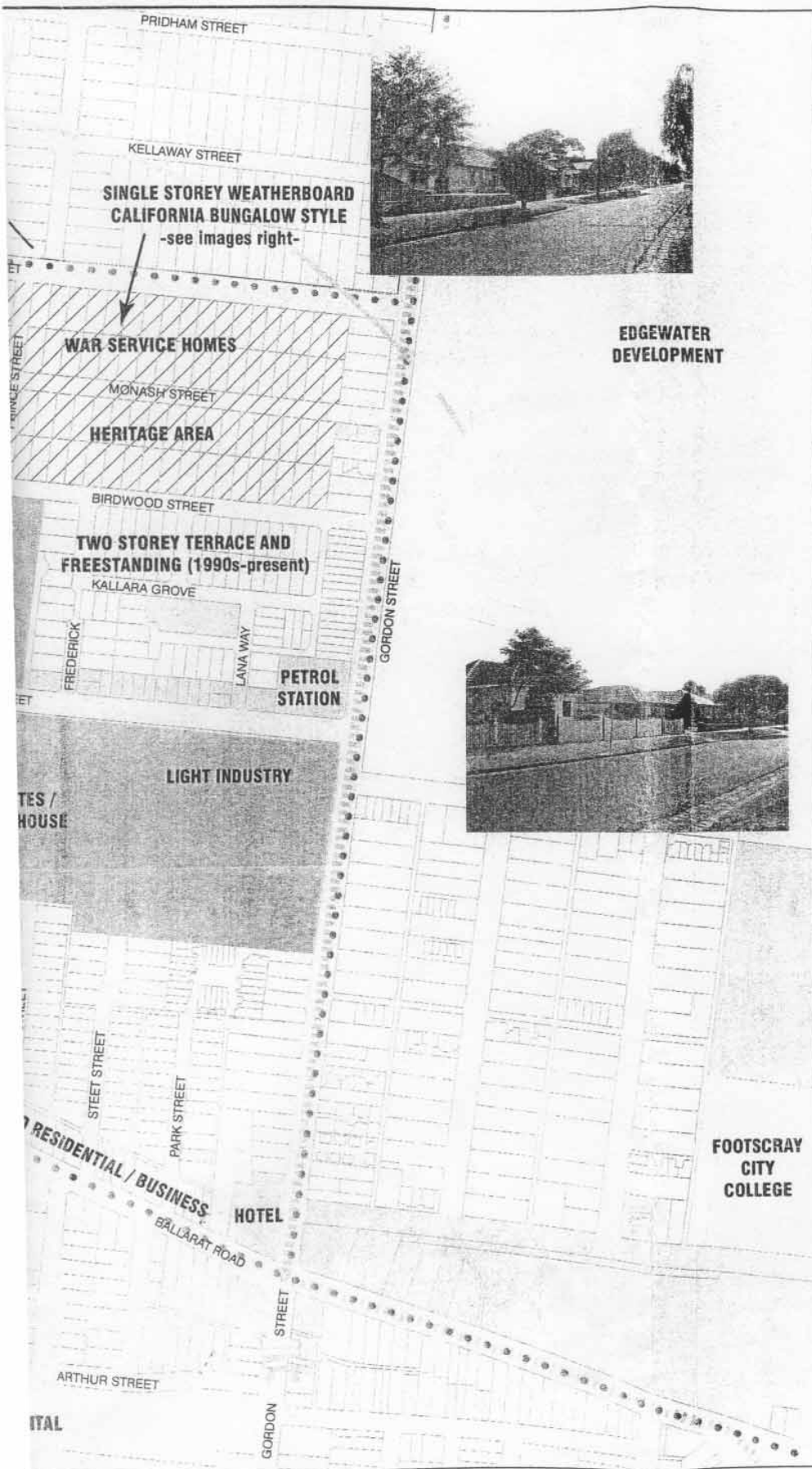


Barrett Reserve (1km)  
Rosamond Bowling Club (1km)





Maribyrnong Secondary College (800m)  
Pipemakers Park (1km)



# LEGEND

- Residential Uses
- Public Uses
- Public Parks
- Industrial Uses
- Mixed Business Uses
- Heritage Area
- Noise interface
- Site Boundary
- Current access point to site
- Shopping
- Place of Worship
- Bus Route
- Tram Route
- Direction of landfall



Racecourse / Showgrounds (3.5km)  
Newmarket Railway Station (4.5km)



## SITE CONTEXT

ALLARA

SCALE = 1:5000

100 0 100 200m

PROJECT NUMBER		REVISION	
303066		00	
DATE	DRAWN	CHECKED	APPROVED
JUL 03	MR	BD	NW

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VUT Footscray Park Campus (1.5km)  
Footscray Town Centre (2km)



## 3.4 Site Opportunities and Constraints

The Site Analysis Plan illustrates the site's opportunities and constraints. They are described further below:

### **Opportunities**

- Existing Roadside Vegetation

While the site itself is largely cleared of vegetation, it is bounded with some established roadside trees in abutting nature strip reserves. These trees vary with regard to both species and maturity but represent a significant opportunity for the site's ready integration to the existing streetscapes of Rosamond Road, Mephan Street and Birdwood Street.

A redevelopment of the site may necessitate the removal of some of these trees so as to provide road access into the development area, as well as cross overs to new allotments. Such tree removal should be minimised as much as practicable.

New roadways within the development area should be designed to reflect the character of the surrounding network with a provision of a nature strip and approximately one street tree to each allotment within this reserve.

- A mixture of buildings types in Mephan Street.

The varied built form and existing industry uses in Mephan Street encourages a mixed land use response on the site's south east corner. This may include multi-level buildings that could accommodate both residential and/or mixed use activities. This mixed use activity will offer suitable transition to the adjoining industry land use.

Internal road networks should allow for pedestrian and cycle permeability from Mephan Street to the provided public park.

- Residential Development on the North Side of Birdwood Street

Development should front and respond positively to the residential interface of Birdwood Street.

- Access

The site is well served with road and footpath access ways on three existing street frontages. These provisions combined with nearby bus and tram services makes Allara a highly accessible site for a variety of people and transport modes. The existing crossing to Mephan Street should be utilised. Vehicle connection to Birdwood Street should also be encouraged.

The development of Allara presents additional opportunities to provide permeability through the site for new and existing residents wishing to access public open spaces or to walk through the site to connect with public transport alternatives. A connection between Birdwood Street and Mephan Street is important to encourage efficient vehicle movement in this neighbourhood.

- Landscape Character

Redevelopment of the site for residential purposes presents an opportunity to remedy this viewscape and enable a better interaction of the site to surrounding interfaces. The removal of the high factory wall will offer improvements to the public realm. New development should offer landscape opportunity along this frontage.

- Housing Diversity

Consistent with recent demographic trends, the housing choices afforded by the site provide for smaller one to two bedroom dwellings. Also offered is apartment style and more traditional large housing. The development plan responds to the changing household size as illustrated in Council's Housing Strategy

### **Constraints**

- Existing Industrial Uses Abutting the Site

As reflected in the Development Plan Overlay (Schedule 4), the most dominant element affecting the site's use for residential purposes is noise effects emanating from abutting land uses east of the development area.

The Schedule recognises the need for noise attenuation measures to enable a residential enjoyment of land within the Allara estate. An appropriate response to this matter will require the construction of acoustic attenuation fencing as well as sensitive building finishes on Lots 1-14 abutting this boundary. This is consistent with the requirements of the overlay, as well as providing certainty for future residents and the neighbouring industrial uses.

The design response (**Figure 3**) responds to the approved 20 Rosamond Road Concept Plan dated February 2002. The key aspects of the design response include:

- Lots and dwellings are designed to front external road frontages and offer an appropriate residential interface to existing housing;
- Lots are designed to orientate views and surveillance from dwellings to the central open space;
- A north-south street will provide connection between Birdwood and Mephan Street to facilitate safe traffic movement;
- All future dwellings on Lots 1-14 will be separated by a minimum of 10m from the east boundary to offer an agreed acoustic buffer to adjoining RMAX industry;
- Central open space will offer visual linkage benefits from external road frontages and act as a central focal point for new community;
- All dwellings will be controlled to encourage garages to be setback behind front alignment of dwellings. Single width garages are proposed for all lots with a frontage width of less than 12m;
- Medium density and multi-storey residential up to 3 storeys is encouraged on Lot 16. This offers a feature corner entry building from Mephan Street;
- Development on Lot 15 will be mixed use to allow for non-residential activity to provide an acoustic buffer to adjoining industry;
- New housing development will encourage garden/landscape opportunity along external road frontages.

The Development Plan establishes the following aims and objectives for use or subdivision of land, construction of a building or construction or carrying out works in the Allara development area. The subdivision, use and development of land must support these aims and objectives.

## **Objectives**

The following objectives reflect the "Requirements for Development Plan" as outlined in the Development Plan Overlay Schedule 4, Maribyrnong Planning Scheme:

## **Land Use**

- Land use is to be residential with Lot 15, to have mixed use component at south east corner on Mephan Street.

## **Lot Size and Layout**

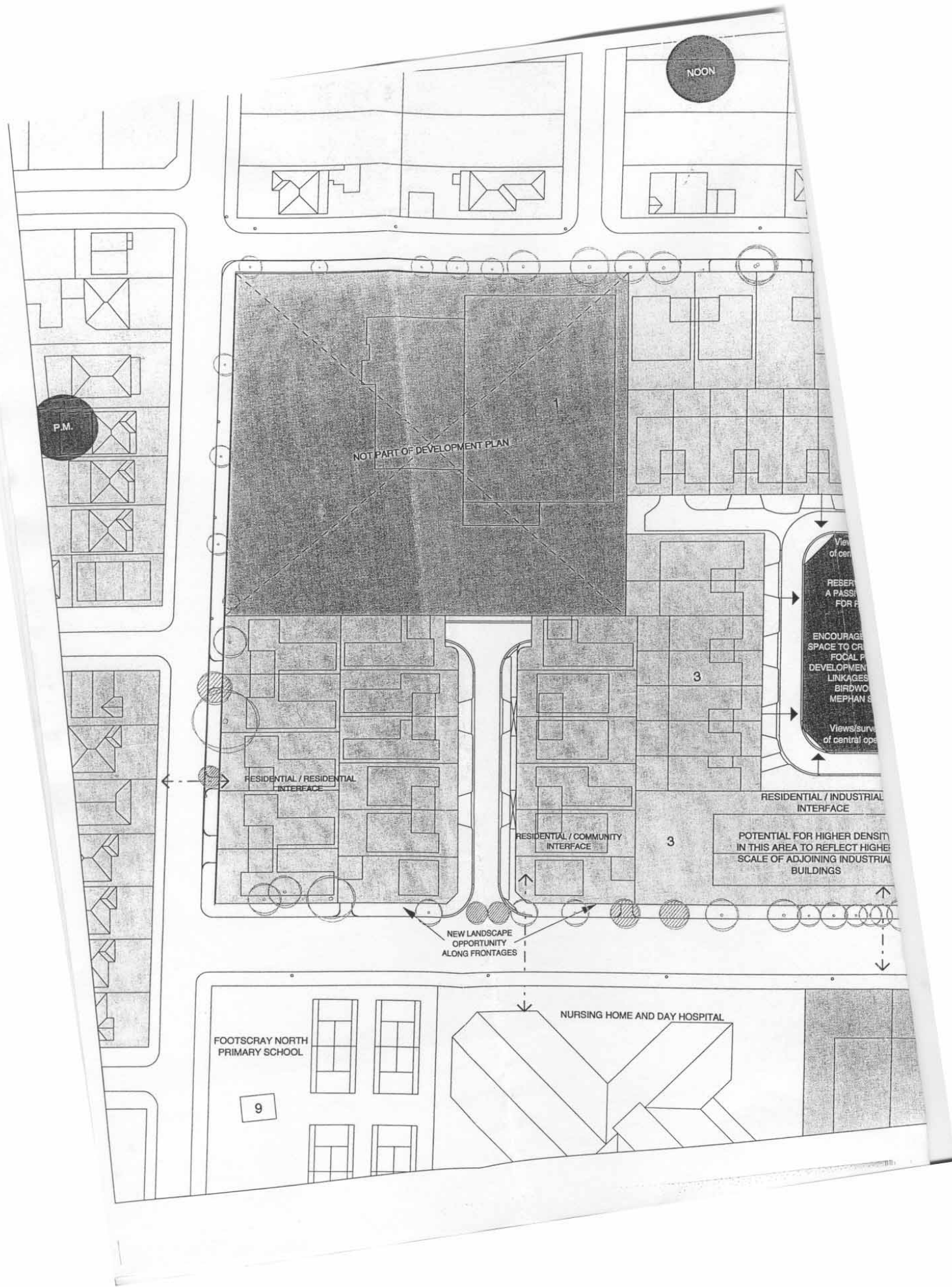
- Lots sizes will vary to offer diversity and choice. This will enable both conventional detached dwellings and medium density developments with some further opportunities for apartments on upper floors to potential mixed use nodes.

## **Private Open Space**

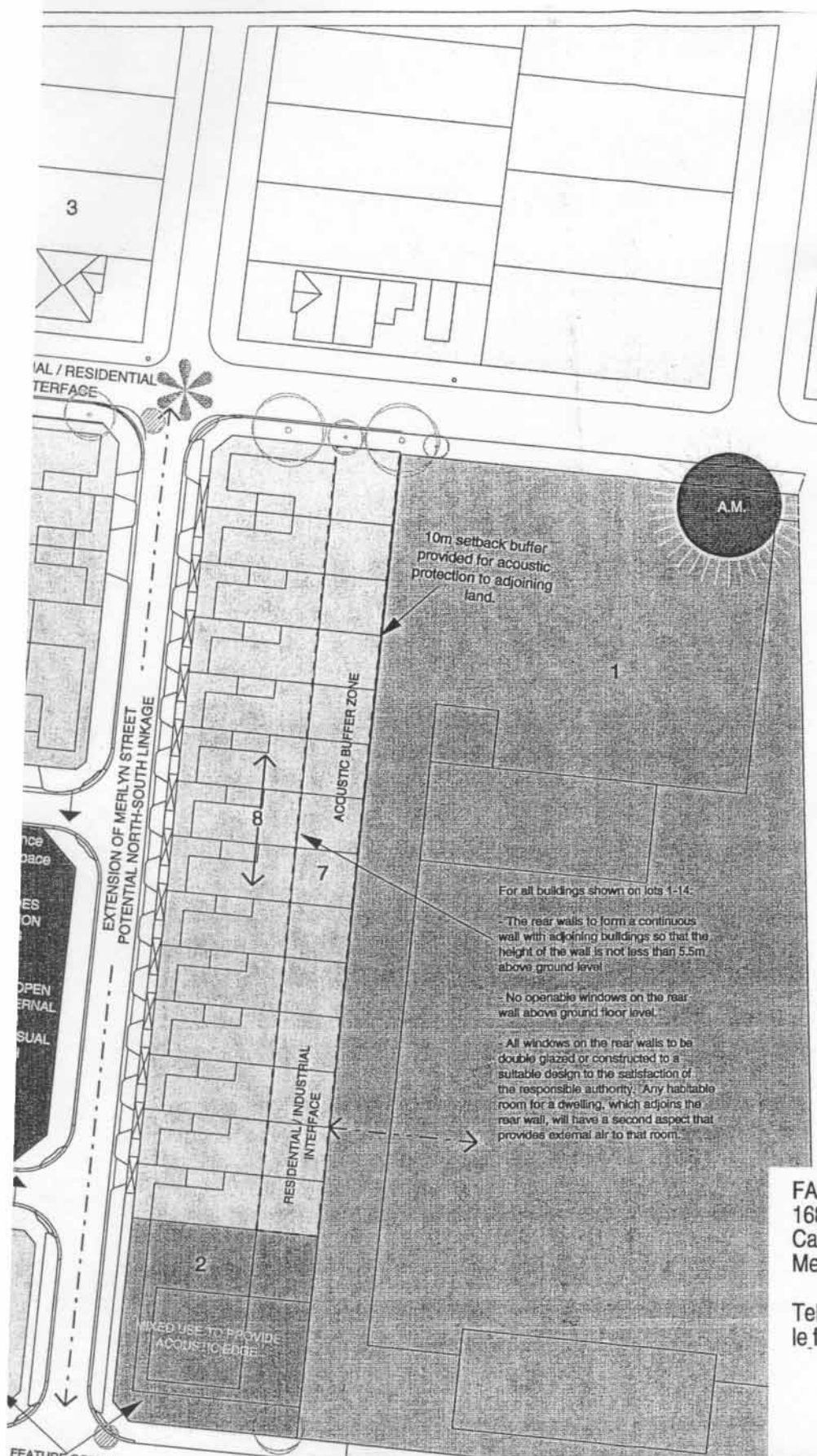
- Private open space is available to the rear of the individual lots.
- An acoustic fence is required along the sites' eastern boundary so as to protect ground floor rooms facing east to this boundary from potential noise effects emanating from adjoining industrial land use. The acoustic fence will offer appropriate acoustic protection as agreed with R-MAX Industries.
- Allotments adjoining this east boundary will also be encouraged to incorporate landscaping which allows for a continuous row of canopy trees so as to obscure views to the acoustic fence and industrial use beyond.

## **Public Open Space**

- A central reserve will provide residents with access to a public open space area within walking distance to each dwelling.
- This reserve area will be required to provide a passive function appropriate to its size and catchment area as well as some small playground facilities.
- The reserve will be located so as to build upon the wider area's public open space networks and allow for ready access from surrounding established areas







- 1 INDUSTRIAL AREA
- 2 MIXED USE AREA
- 3 RESIDENTIAL AREA
- 4 PUBLIC USE AREA
- 5 PUBLIC OPEN SPACE
- 6 FOOTPATHS
- 7 ACOUSTIC BUFFER ZONE
- 8 TWO STOREY BUILT FOR WITH SHARED WALLS TO PROVIDE FOR ENERGY EFFICIENCY
- 9 TWO STOREY BUILT FOR WITH SHARED WALLS TO PROVIDE FOR ENERGY EFFICIENCY
- EXISTING TREES
- TREES TO BE REMOVED
- A FULLY MOUNTABLE ROUNDABOUT OR OTHER SAFETY DEVICE TO BE CONSTRUCTED TO COUNCIL'S SATISFACTION

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ALLARA

## DESIGN RESPONSE

ALLARA

SCALE

1:1000 @ A3

PROJECT NUMBER  
303066

DATE  
MAR 04

DRAWING NUMBER  
PD 03

DRAWN  
RC

CHECKED  
MJ

REVISION  
06

APPROVED  
BD

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FIGURE

## **Streetscape Character**

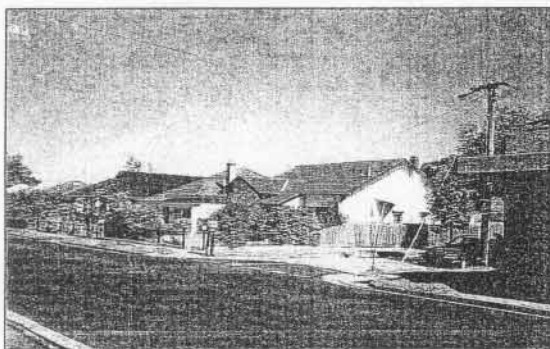
- Roads will be designed and constructed to accord with the predominant streetscape character. This includes fully constructed pavements and edges, nature strip reserves and footpaths.
- Nature strips will be required to be landscaped in accordance with an overall Landscape Plan.
- Fences will be required to be generally of a low nature to encourage visual permeability to front gardens.

## **Road and Pedestrian Network**

- Principal road access will be from Rosamond Road into Mephan and Birdwood Streets.
- The road and pedestrian network will enable easy access to the internal network and open space.
- Road and pedestrian networks shall be designed so as to provide efficient linkages between the stages of development.
- A fully mountable roundabout will be constructed at the intersection of Merlyn and Birdwood Streets.

## **Service Provisions**

- Each new allotment within the site will be required to be connected to all urban reticulated services.
- Typically, utility services will be provided underground to new residences along all new roads.
- The development of land for residential purposes should provide no adverse impacts to existing service networks or systems.



Housing mix looking south along Rosamond Rd, opposite the subject site

# DEVELOPMENT PLAN REQUIREMENTS

5

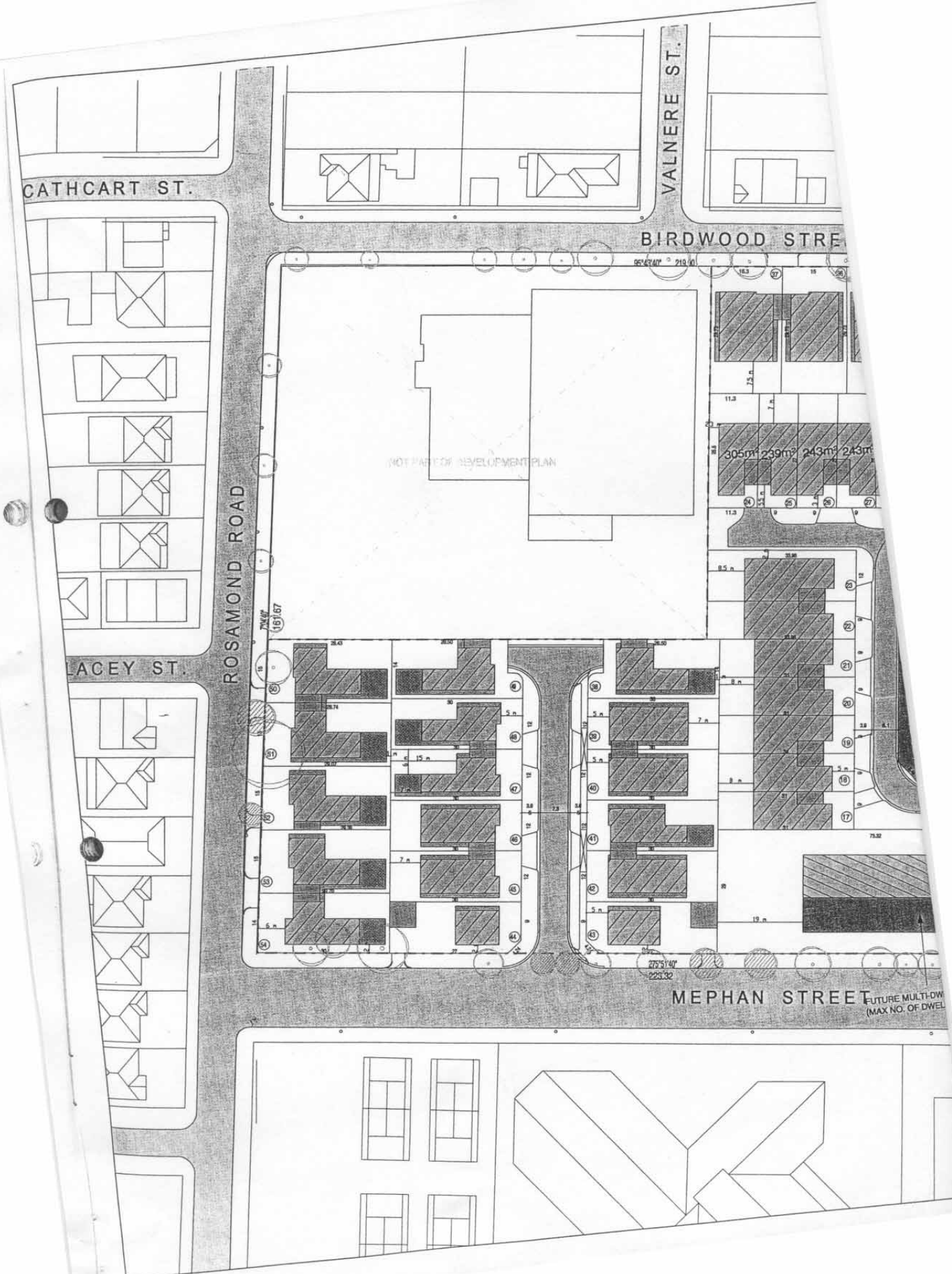
## Other Requirements and Conditions of the Development Plan

These are specific requirements to be shown in the Development Plan as required by DPO Schedule 4 in the Planning Scheme. Please refer to **Figure 4**. The requirements are shown in bold.

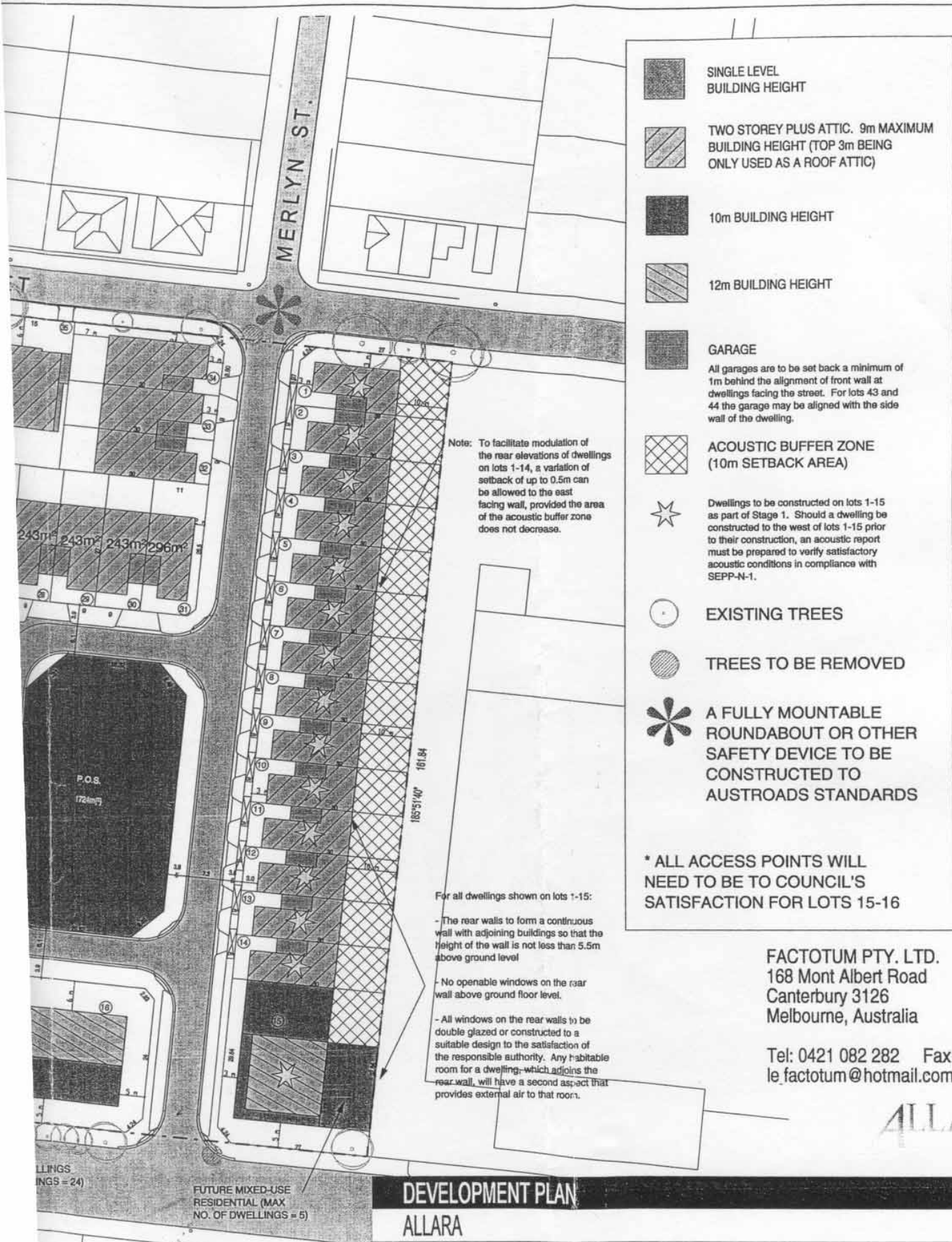
The Development Plan must show:

<b>A detailed Site Analysis Plan showing existing conditions on the subject land and adjoining properties.</b>	Refer Figure 2
<b>The proposed linkages between the land and adjoining land.</b>	<p>The Development Plan proposes a simple road network pattern that will link with the three existing street frontages.</p> <p>The plan proposes a north-south road to connect Birdwood Street to Mephan Street and provide residents with alternative access points to the estate. Significantly, this road is proposed in alignment to the existing Merlyn Street which extends north from the development area. This road represents the site's most important link to its surrounds and it will offer existing residents greater movement options in their local area. It will offer physical and visual linkage to the surrounding area.</p> <p>An internal access road will extend around the proposed local park. This road while providing access to residential allotments will also ensure that the park itself will be provided with passive surveillance and accord with principles of safety through environmental design.</p> <p>A separate access place approximately 65m into Mephan Street from Rosamond Road will provide local access for residents of this street.</p> <p>These streets offer vehicle and pedestrian connection and links to the surrounding area.</p>
<b>The subdivision layout generally in accordance with the Concept Plan.</b>	The Development Plan includes a subdivision layout that is consistent with the Concept Plan. It retains a total of 54 lots with Public Open Space.
<b>The proposed road network and hierarchy, and pedestrian and bicycle paths.</b>	<p>As detailed above, all new roads will provide local access for residences. These will be constructed and provided in character to existing roads surrounding the site and will consist of footpaths and nature strips.</p> <p>To facilitate pedestrian movement and enhance safety, pathways have been provided along each street frontage.</p>
<b>The provision of public open space and road verges.</b>	<p>The development plan shows a local park in accordance with the Concept Plan which will be highly accessible from within the development area and surrounds. The park offers an approximate area of 1724m<sup>2</sup>.</p> <p>Each newly created road will be provided with nature strips and footpaths within road verges, excepting those surrounding the local park. It is submitted that pedestrian movements on these streets are likely to traverse the park itself and that additional footpaths would be superfluous to residents' requirements.</p>
<b>The dimension and area of all new allotments.</b>	The dimensions and area for each proposed allotment within the site <b>Figure 5</b>

11







CATHCART STREET

VALNERE STREET

BIRDWOOD STREET

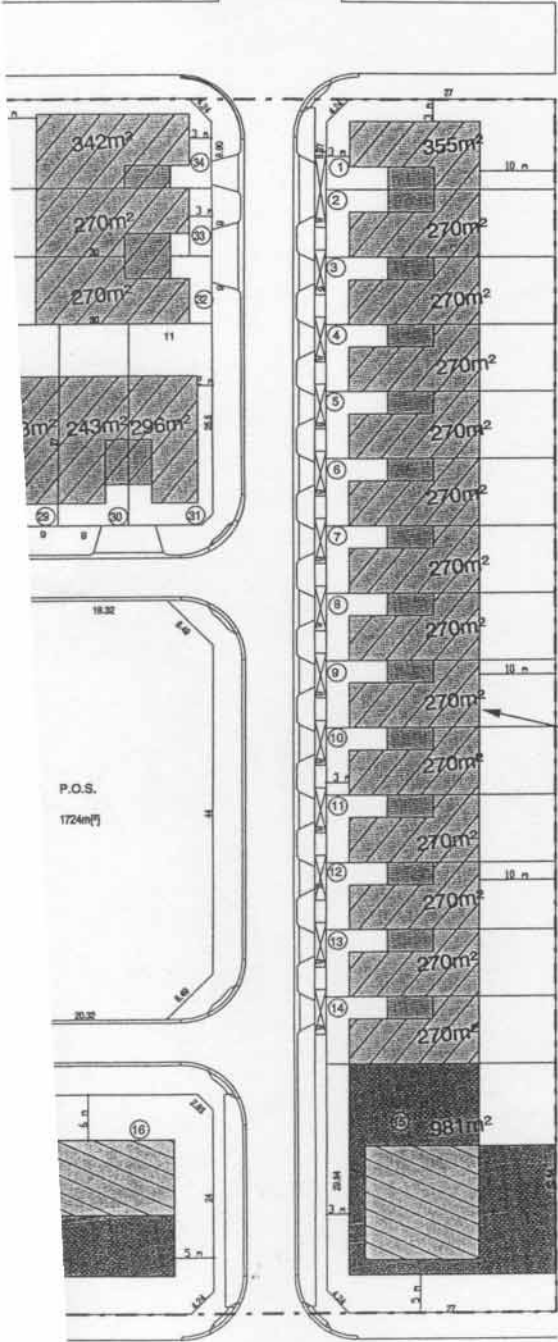
NOT PART OF DEVELOPMENT PLAN

ROSAMOND ROAD






MEPHAN STREET



MERLYN STREET



## LEGEND

-  SINGLE LEVEL BUILDING HEIGHT
-  TWO STOREY BUILDING HEIGHT
-  10m BUILDING HEIGHT
-  12m BUILDING HEIGHT
-  GARAGE

Note: To facilitate modulation of the rear elevations of dwellings on lots 1-14, a variation of setback of up to 0.5m can be allowed to the east facing wall, provided the area of the acoustic buffer zone does not decrease.

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ALLARA

## BUILDING ENVELOPE PLAN

ALLARA

SCALE

1:1000 @ A3

PROJECT NUMBER  
303066

DATE  
11/01/04



DRAWING NUMBER  
PD 01

DRAWN  
RC.

CHECKED  
MJ

REVISION  
03

APPROVED  
BD

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FUTURE MIX-USE/  
RESIDENTIAL

Two-dimensional building envelopes generally in accordance with the envelopes shown on the Concept, and for all new allotments.	A two-dimensional Building Envelope Plan has been prepared and is provided for reference in <b>Figure 4</b> . This plan is duplicated in the <i>Urban Design and Siting Guidelines</i> , which identifies a maximum building height of 9 metres for residential lots and allows in part, a maximum height of 12m for mixed use and medium density lots on lot numbers 15 and 16.
The provision for major physical infrastructure to accommodate the subdivision including the provision of water, sewerage, drainage, electricity, gas and telecommunications.	<p>The layout of each allotment is suitable for each to be connected to urban utilities. Services will be predominantly contained in easements located in the rear or side of open space areas and where these will be accessed from within the frontage, (ie electricity and telecommunications), services will be undergrounded on all new roads. A report by Earth Tech Pty Ltd confirms the adequate supply of infrastructure services.</p> <p>New lots fronting existing roads will enjoy existing access rights to overhead electricity power poles. These proposed lots can accordingly connect to these services.</p>
The maximum height of buildings to be not more than shown on the Concept Plan, the top 3 metres being only used as a roof attic.	Refer to <b>Figure 5</b>
<p>For all buildings shown on Lots 1-14 on the Concept Plan:</p> <ul style="list-style-type: none"> <li>- The rear walls to form a continuous wall with adjoining buildings so that the height of the wall is not less than 5.5 metres above ground level</li> <li>- No openable windows on the rear wall above ground floor level.</li> <li>- All windows on the rear walls to be double glazed or constructed to a suitable design to the satisfaction of the responsible authority. Any habitable room for a dwelling, which adjoins the rear wall, will have a second aspect that provides external air to the room.</li> </ul>	A specific note is included on the Development Plan to reinforce these acoustic buffer requirements to the adjoining R-Max industry site. They allow for variations if an acoustic report is prepared by an authorised acoustic engineer showing compliance with SEPP N-1. These requirements are also reflected in Part 2.
Location of garages/carports and car spaces on site so that the structures are largely located to the rear of the sites or recessed behind the main building line so as not to dominate the streetscape. No garages, carports or car spaces to occupy more than 40 percent of the allotment's frontage.	<p>The Development Plan and Building Envelope Plan (<b>Figures 4 and 5</b> respectively) identify garages to be setback a minimum of 1 metre behind the front wall alignment of dwellings facing the street.</p> <p>On all lots less than 12m, single garages are proposed to minimise dominance of garages to the street. On Lots 38-42 and 45-48 double space garages are proposed for 12m wide lots.</p>
Tree protection.	Subject to separate S.173 agreement.

<p><b>The maximum number of dwellings on the sites identified on the Concept Plan as Possible Apartments Site and Possible Mixed Use Site.</b></p>	<p>The maximum number of dwellings in the proposed building on Lot 16 will be 24. This will comprise a mixture of 1, 2 and 2 plus study bedroom apartments.</p> <p>The maximum number of dwellings in the proposed building on Lot 15 will be 9. This will comprise a mixture of apartment sizes and some potential mixed use activities as annotated on the Allara Development Plan. Refer to <b>Figure 4</b></p> <p>Separate to this process it has been demonstrated by the proponent that the dwelling numbers and configurations on these sites can work, while providing an end product of a high standard.</p>
<p><b>Access to sites and turning movements for service and emergency vehicles.</b></p>	<p>The Development Plan shows a clear road network that will provide appropriate turning spaces for vehicles. This is reflected in the Traffic Analysis report prepared by the Traffix Group Pty Ltd 2002 which found that:</p> <ul style="list-style-type: none"> <li>- The internal road cross-sections and intersections are provided in accordance with Rescode and current practice and will easily accommodate garbage and service vehicles.</li> </ul>
<p><b>The staging of development so that Lots 1-15 of The Concept, is developed first to provide a noise barrier between the industrial activity to the east and the remainder of the land.</b></p> <p>Alternatively, if development on Lots 1-15 has not been fully completed, the proponent of a development of a lot on the land to the west of the prolongation of Merlyn Street must submit an acoustic report, which identifies sound levels at critical points to show compliance with the requirements of SEPP Noise N-1. This report must be authorised by an acoustic engineer who is a member of the Institute of Engineers Australia and be to the satisfaction of the Responsible Authority. Such report may be used by the responsible authority as proof of compliance with SEPP Noise N-1 for other developments further to the west of the lot tested.</p>	<p>It is proposed to construct dwellings on Lots 1-15 as part of Stage 1 so they can form a noise barrier to industry located to the east of the site. Should development of dwellings on Lots 1-15 not be completed prior to dwellings being constructed on other lots within the development area, an acoustic report will be required to demonstrate that a use of these dwellings will not be compromised by industrial uses east of Lots 1-15. This is annotated on the Development Plan.</p>



<p><b>Supporting technical information including traffic assessment, provision of services and drainage assessment.</b></p>	<p>A public realm plan has been prepared to highlight the private / public interface of the development plan. <b>(Figure 6)</b></p> <p>Separate documents are submitted in conjunction with this Development Plan to demonstrate the plan's ability to resolve these requirements. These include:</p> <ul style="list-style-type: none"> <li>&gt; Traffic Analysis Report – Traffix Group Pty Ltd, 2002 which found that: <ul style="list-style-type: none"> <li>- The traffic generated as a result of the proposed development will be distributed evenly onto the surrounding road network and will not have a detrimental impact on it;</li> <li>- The internal road cross-sections and intersections are provided in accordance with Rescode and current practice and will easily accommodate garbage and service vehicles;</li> <li>- One on-site car space should be provided for every one and two bedroom dwelling;</li> <li>- Two on-site car spaces should be provided for every dwelling with three or more bedrooms;</li> <li>- More than enough on-street parking will be provided to satisfy the visitor requirement and expected visitor demand;</li> <li>- Some of the parking demand for any potential mixed-use development could be met by the available on-street parking in Mephan Street; and</li> <li>- There are no traffic engineering reasons why a permit should not be granted for the proposed 54 lot subdivision at 20 Rosamond Road, Maribyrnong.</li> </ul> </li> <li>&gt; Servicing report – Earth Tech Pty Ltd 2002, which found that: <ul style="list-style-type: none"> <li>- Drainage – The site is not subject to flooding and the City of Maribyrnong is the Responsible Authority for internal stormwater drainage with Melbourne Water responsible for main drainage. It is considered that a use of the site will pose no adverse affects to existing drainage systems.</li> <li>- Sewerage Facilities – City West Water is the Responsible Authority for sewerage facilities and advice has been received demonstrating that the existing system has spare capacity to accommodate the proposed development of land.</li> <li>- Water Supply Facilities – City West Water has advised that the existing water system has spare capacity to accommodate the proposed development.</li> </ul> </li> </ul>
---	---

	<ul style="list-style-type: none"> <li>- Telecommunications – Through the principal authority of Telstra, each allotment can be readily provided with telecommunications with shared trenching of services provided by the developer.</li> <li>- Gas Supply – TXU Networks is the authority responsible for the provision of gas reticulation and advice has been received indicating that the site is located adjacent to a high pressure pipeline in Rosamond Street and can be readily connected to this service.</li> </ul> <p>Electricity Supply – AGL is the authority responsible for the provision of electricity supply to service the proposed development. Advice has been received indicating that new allotments to existing streets will enjoy existing access rights to overhead power lines and all allotments facing new roads will be required to underground reticulated electricity supplies. The site can be readily connected to these services.</p>
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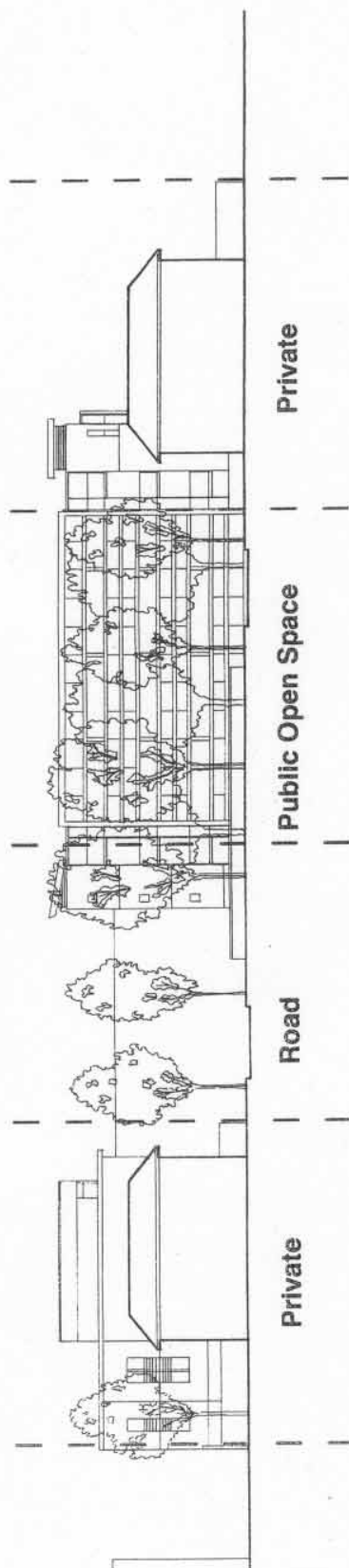


Figure 6 - Public Realm Plan



# CONCLUSION

6

This Development Plan provides a framework for the development of a high quality, sustainable residential precinct.

It has been prepared in accordance with the Development Plan Overlay (Schedule 4) of the Maribyrnong Planning Scheme which requires that a Development Plan be prepared to co-ordinate the development of the site for residential use prior to a permit being granted to subdivide or develop the land.

A lengthy process has taken place between Council and the developers to ensure the Development Plan provides a clear Statutory basis for consideration of future permit applications.

TRACT CONSULTANTS PTY LTD

PLANNERS  
URBAN DESIGNERS  
LANDSCAPE ARCHITECTS

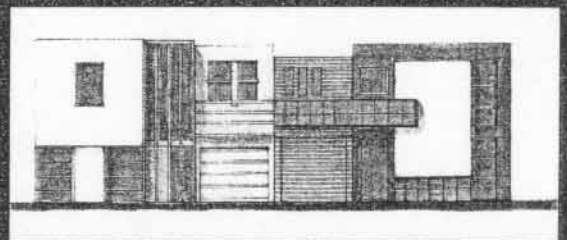
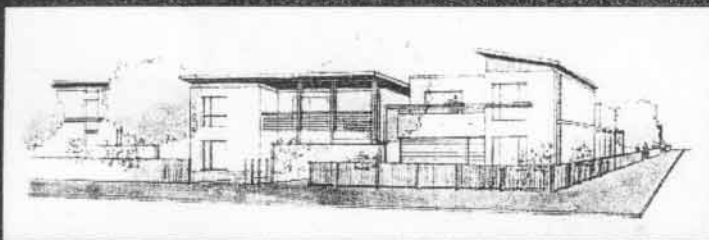
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TRACT

## **PART 2:**

# **Urban Design & Siting Guidelines**



**Prepared by Tract Consultants Pty Ltd**

Reference: 303066\_10

In Association with:

Factotum Pty Ltd

March 2004

ALLARA

# CONTENTS

Glossary and Terms	3
1 Introduction	3
2 Building Siting and Height (refer to Building Envelope Plan)	4
3 Architectural Design requirements	6
4 Fencing	9
5 External structures and outbuildings	10
6 Garages, vehicle access and parking	11
7 Landscaping / Trees	12
8 Energy Efficient Design	13
9 Water tanks	14
10 Acoustic Protection	15
11 Council assets	16
12 Variations	16
13 Verification	16
14 Appendix 1 - Building Envelope Plan	
15 Appendix 2 - Allara Development Plan	

This document is to be read in conjunction with the:

1. Development Plan for Rosamond Rd, Birdwood Street and Mephan Street (August 2003)
2. The Rosamond Road Concept Plan (Feb 2002)

## Glossary and Terms

### Habitable Room

"Habitable room means a room used for normal domestic activities, and-

- (a) includes a bedroom, living room, lounge room, kitchen, music room, television room, dining room, sewing room, study, play room, family room, and sun room; but
- (b) excludes a bathroom, laundry, water closet, pantry, walk-in-robe, corridor, hallway, lobby, photographic darkroom, clothes drying room, and other spaces of a specialised nature occupied neither frequently nor for extended periods."

### Timing of development and plans

Plans must be submitted within 6 months following the settlement date for your land and you must commence construction of your dwelling, garage and fence within 12 months following the settlement date. The construction of the dwelling, garage and fence must be completed within 12 months from the commencement of construction.

# INTRODUCTION

1

The infill development of Allara - Rosamond Road, Birdwood Street and Mephan Street, Maribyrnong ("Allara"); is to be developed as a new housing community to offer new lifestyle opportunities in this inner western region of Melbourne.

The high quality of housing to be developed at Allara will create an ambience and character which future residents can take pride of in their new neighbourhood.

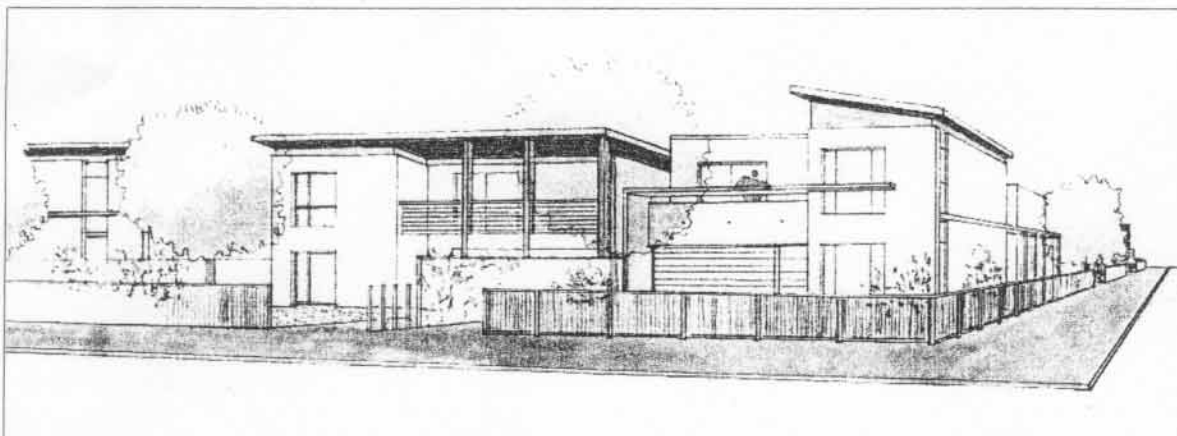
Should a proposal for development vary from these guidelines it will be necessary to first obtain consent from the Council and will only be approved if it meets the spirit of the guidelines (see section 12 of this document for Variations). Variations to these guidelines are likely to be discouraged by the Council.

The guidelines have been formulated to ensure that a consistent building design is achieved within the development by establishing a framework for the design of private dwellings. In addition, they outline the design elements which are important to interface with the public realm.

These guidelines encourage an identifiable commonality in forms and materials that enhance harmony in the development without unnecessarily restricting the design of any individual building.

These guidelines also encourage innovative design of dwellings and include setback areas from lot boundaries to provide acoustic protection from nearby land. The effective siting, design and construction of new dwellings in accordance with these guidelines will provide future residents with ultimate acoustic and amenity protection.

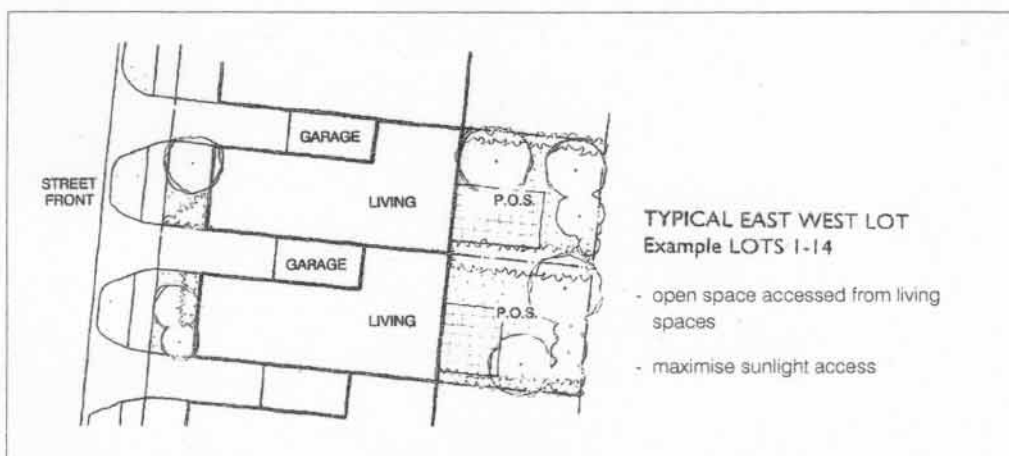
The guidelines are developed to protect your property and encourage a harmonious and comfortable living environment. It is recommended that you engage a fully qualified architect in the design of your dwelling, who will make best use of these guidelines in providing a quality product that fulfils not only Council's objectives, but gives you, the owner, a sustainable product for the future.



Architectural impression of Rosamond Road frontage.

## Lots 1 - 14 & 17 - 54

- 2.1 The use of each lot must be residential and only one dwelling will be erected and used on each allotment shown on the *Building Control Plan*.
- 2.2 The minimum area of a dwelling for any lot must be 120 sq metres excluding the garage/carport or verandah.
- 2.3 For each allotment on the Building Envelope Plan:
  - dwellings must be located within the nominated envelope and set back at least the minimum distance from the frontage.
  - garages must be setback at least 5.5 metres from the front boundary and must not be forward of the dwelling setback.
  - shared walls are encouraged on side boundaries, forming a terrace house effect.
  - For side setbacks refer to Rescode Provisions, the Building Code of Australia and Building Regulations or any other superseding design code.
- 2.4 Architectural features such as fins, blades and light well projections may extend above the second storey roofline provided that any space created is not used for habitation.
- 2.5 Prefabricated, reconstructed or transportable dwellings cannot be erected.  
(Note: For specific controls on garages refer to section 6 of these guidelines)
- 2.6 The following structures attached to the house are allowed to extend beyond the building envelope:
  - Verandahs, porches, balconies, bay windows, oriels and other forms of articulation provided they do not encroach more than 1 metre into the front setback.
  - Pergolas and decks provided they are situated on the side and/or rear of the dwelling.
  - Non-habitable outbuildings, subject to siting requirements (see section 5 of these guidelines).



- 2.7 Lots 1-14 must not exceed a height of 9 metres, and vary between 5.5 - 9 metres.
- 2.8 The maximum height of buildings on Lots 17-54 shall be 9 metres.
- 2.9 Acoustic measures for Lots 1-14, shall comprise those attenuation matters detailed in Section 10.2 of these guidelines.

## Lots 15 & 16 – Apartments and Mixed Use

- 2.10 The position of first floor windows of the dwelling should aim to limit views into secluded private open space and habitable rooms and/or windows.



Architectural impression of proposed development at lots 15, 16 and 1-14 (Conceptual only).

- 2.11 The maximum number of dwellings in the proposed building on Lot 16 will be 24. This will comprise a mixture of 1, 2 and 2 plus study bedroom apartments.
- 2.12 The maximum number of dwellings in the proposed building on Lot 15 will be 5. This will comprise a mixture of apartment sizes and some potential mixed use activities as annotated on the building envelope plan.
- 2.13 The maximum height of buildings on lots 15 & 16 shall be 12 metres with the third storey proposed in accordance with the Building Envelope Plan.
- 2.14 Acoustic measures for east facing dwellings on lot 15, shall comprise those attenuation matters detailed in section 10.2 of these Guidelines.



Conceptual elevation of Mephan Street entrance.

# ARCHITECTURAL DESIGN REQUIREMENTS

3

The intent of these design requirements is to encourage an architectural design consistency for the new housing in the Allara development.

The key elements of building design encouraged at Allara will include:

## Elevations

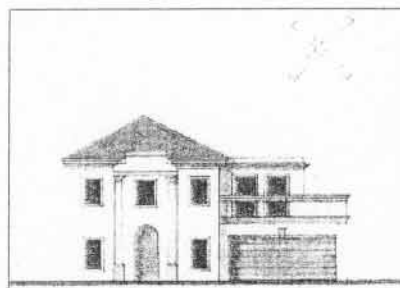
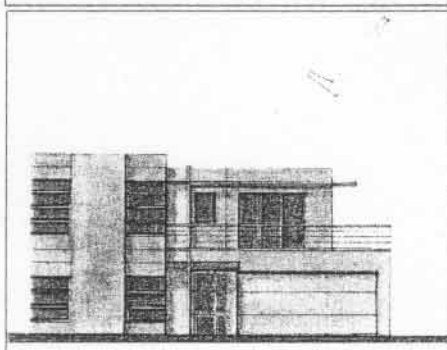
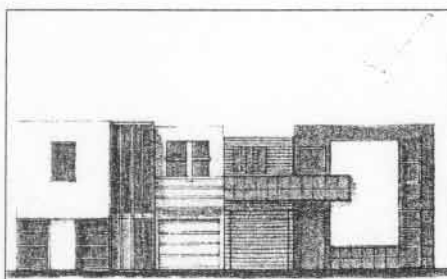
The style and architectural design of houses at Allara is a key component of the approval process.

- 3.1 Dwelling entrances should be clearly visible from the street.



- 3.2 Exposed plumbing, except for roof plumbing, must not face the street.

- 3.3 Articulation of front and side elevations visible from the street



- 3.4 Contemporary and modern forms will be encouraged whilst mock historical reproduction styles are not permitted, as shown above.

- 3.5 It is essential that corner buildings in particular demonstrate articulation and movement. Corner buildings require distribution of various building elements to create attractive forms.



# ARCHITECTURAL DESIGN REQUIREMENTS

3

## External Colours

- 3.6 External house colours are to be predominantly selected from the neutral range in muted tones i.e. creams, greys, browns. Limited use of vivid colours for highlighting individual elements is permitted.
- 3.7 Materials and colours used on carports, fences, gates, garages, verandas and roofs must compliment those of the dwelling.

## External Wall Materials

- 3.8 The style that will be encouraged is one which provides for contemporary or modern buildings that will allow for the optimal use of modern materials in a manner that creates variety and interest in the space created between the facades of buildings.

In order to modulate the elevations as viewed from a road or open space, materials should reinforce the articulation of front and side elevations.

Materials should be a combination of but not limited to:

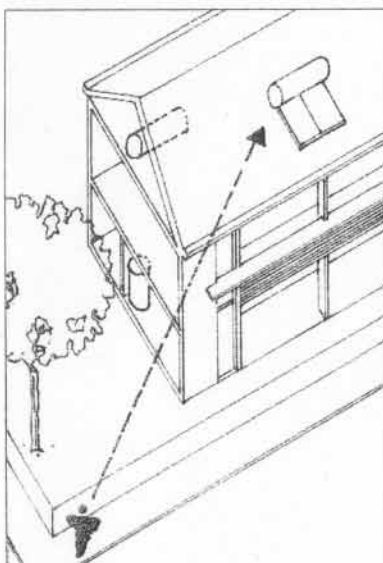
- Face brickwork;
- Rendered or bagged with a painted surface;
- Face painted;
- Painted weatherboards - this material should be generally used above ground level;
- Natural stone, timber or tiles.

A combination of brick and rendered finish may be considered, however the use of external wall materials will need to be considered on the basis of architectural merit.

The quality of the brick work / external cladding used is important, and a palette must be provided to Council with the verification notice. The use of "reproduction styles" is not permitted.

## Roof Design

- 3.9 Predominant roof styles are to be hipped or gable. Roof pitch should be between the angles of 22 degrees and 35 degrees. As the roof pitch increases the eaves width can be reduced.
- 3.10 Solar water heaters are to be located on roof pitches that minimise their visibility from public areas.



- 3.11 Varying roof forms are not discouraged but all designs, including those of contemporary architecture must offer high quality presentation and articulation to the streetscape.



# ARCHITECTURAL DESIGN REQUIREMENTS

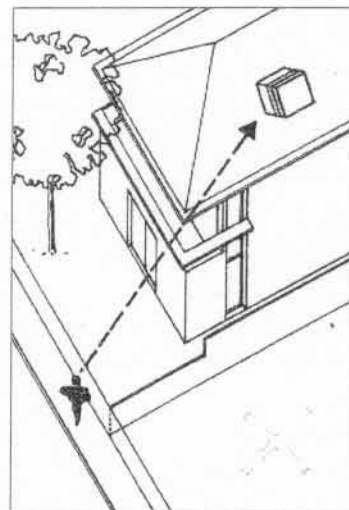
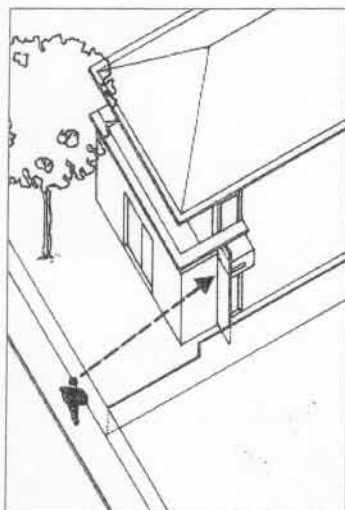
3

3.12 Roof finishes must be in either colourbond metal roofing, cement, low profile terracotta or slate tiles.

## Ancillary Structures

3.13 Air conditioning units, satellite dishes and similar structures must be concealed and located below the eaves line.

- Note all satellite dishes over 1.2m in diameter that are visible from adjoining property require planning approval in accordance with Clause 52.04 of the Maribyrnong Planning Scheme.

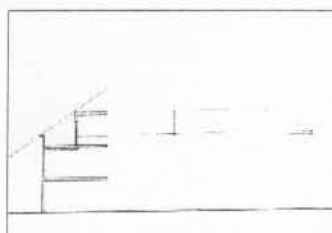


## Roof Attics

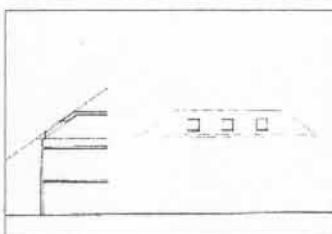
3.14 The maximum height of buildings to be not more than shown on the development plan, the top 3 metres may be used as a roof attic.

For all other dwellings, a maximum height of 9 metres, the top 3 metres being only used as a roof attic

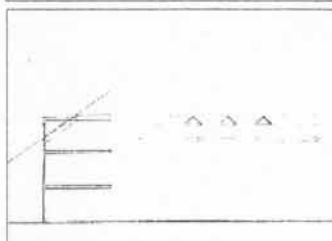
- Habitable rooms above 6m must be contained in the roof space



Attic set back from front facade within 45 degree creating terrace area



Attic illustration of possible skylight within roof volume

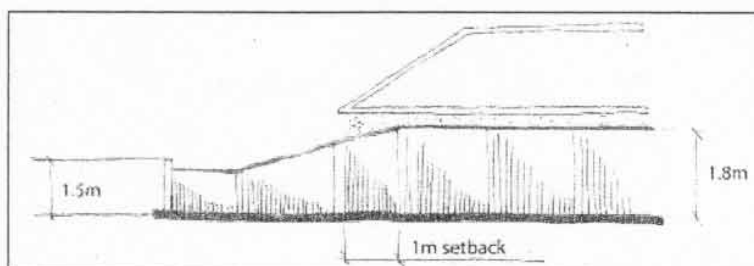
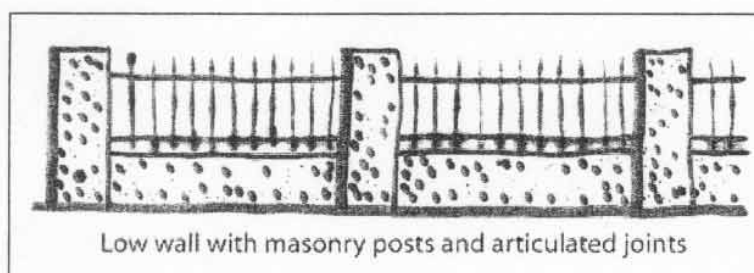
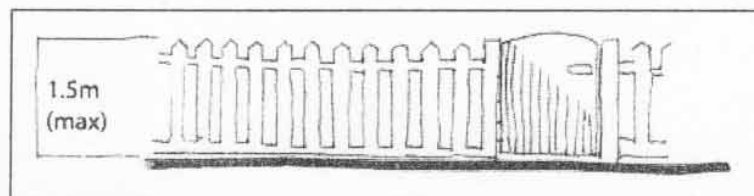
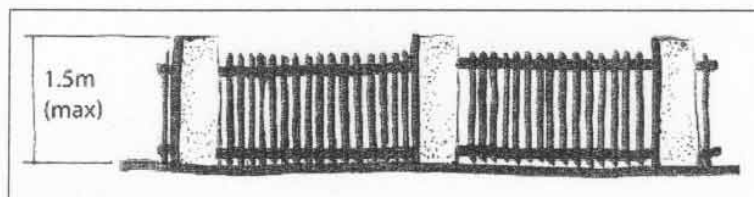


Attic illustration for dormer windows principle. Note: The sum of the dormer window length should not exceed one third of the length of the eaves

# FENCING

4

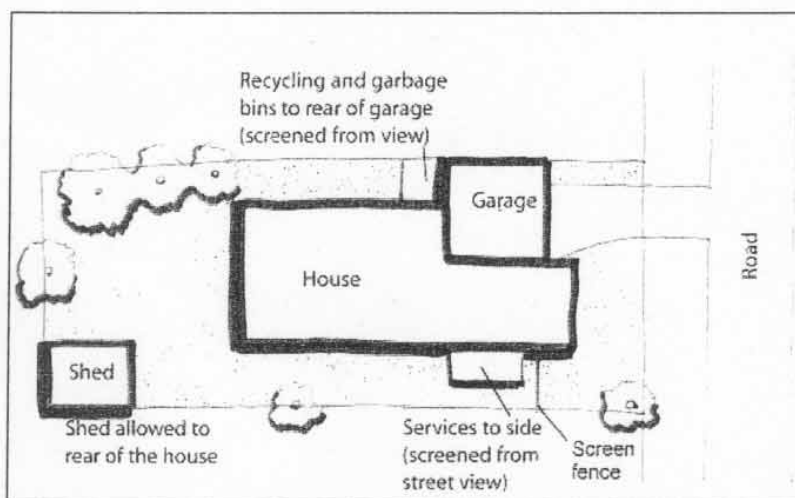
- 4.1 Front fences for detached houses within the Allara Estate are optional.
- 4.2 The maximum height of a front fence shall be 1.5 metres. The design of front fences should complement the design of the dwelling.
- 4.3 Side and rear fencing shall be a maximum height of 1.8 metres. Side fencing shall reduce in height to meet the height of the front fence.
- 4.4 The position and design of letter boxes should be incorporated into the front fence.
- 4.5 The construction of an acoustic fence at the rear boundary of Lots 1-14 must be designed to protect the ground floor rooms facing east. Owners are required to maintain the fence in accordance with the agreement.
- 4.6 As illustrated below, fences must be constructed with bricks and masonry, timber pickets, iron posts or with a variety of these materials. In any case, 50% of the fence must be visually transparent from the street.
- 4.7 Front gates should be constructed with materials that are complementary to those used in the front fence. In the case of brick and/or iron fencing, the gate should be constructed of iron and be 50% transparent. In the case of pickets, the gate should be constructed of timber and also provide 50% transparency.



## EXTERNAL STRUCTURE & OUTBUILDINGS

5

- 5.1 External fixtures, such as clotheslines, antennas, satellite dishes, hot water systems and heating /cooling equipment must not be seen from the street. In all instances, such fixtures are to be screened from public view.
- 5.2 Non-habitable outbuildings, such as sheds, are permitted outside the approved building envelope behind the rear wall of the house.
- 5.3 Storage areas for garbage and recycling bins must be provided. These must also be screened from the street.
- 5.4 Utility services such as gas, electricity and water metres should be located so as to be accessible by service authorities in an absence of the dwelling occupier. Such facilities should be located discretely within garden beds (water), recessed within the verandah (electricity) or to the side of the dwelling (gas).



## GARAGE, VEHICLE ACCESS & PARKING

6

- 6.1 Only one standard and single width crossover per allotment is permitted. Crossovers are encouraged to be on the north side of the lot to improve solar access.
- 6.2 All driveways must be sealed and paved.
- 6.3 Garages must be integrated into the dwelling or freestanding structures. The garage must not dominate the frontage and garage doors cannot be a prominent feature.
- 6.4 Lots 43 and 44 have been annotated with side access to garages due to the location and orientation of these lots.
- 6.5 Materials and colours used should compliment the dwelling. The garage roof is to compliment the roof style of the dwelling.
- 6.6 Garages or carports shall be setback at least 5.5 metres from the front boundary, and at least 1m behind the front of the building.
- 6.7 The internal dimensions must accord with Rescode which requires the following:
- Single garages – 3.5m x 6m
  - Double garages – 5.5m x 6m
  - Triple garages will not be permitted
  - Adequate storage facilities within garages for bins etc need to be incorporated
- 6.8 A minimum of 2 off-street parking spaces must be provided except for dwellings on Lots 15 & 16. One of the car spaces should be covered. A second tandem space can be located in the driveway. The location of garage or carport must be located in accordance with the garage notations on the approved Building Envelope Plan, except with the written consent of Council.

- 7.1 The introduction of new landscape is encouraged within the front setback to improve presentation of buildings particularly as they present to the street. The inclusion of grassed areas and trees in the front setback areas is important to improve the streetscape appearance.
- 7.2 Landscaping should enhance the house and streetscape as well as add to the comfort and privacy of indoor and outdoor living. Special attention should be given to outdoor living areas where deciduous shade trees and evergreen trees can combine to achieve high quality, useful outdoor spaces and a comfortable microclimate all year round.
- 7.3 The inclusion of trees in the rear yards and adjacent to boundaries offers residents shading and privacy screening benefits which are important to maintain a high quality and comfortable living environment. Landscaping combined with good solar orientation design will provide an improved energy efficiency of the house by reducing the need for heating and cooling.
- 7.4 The inclusion of trees and landscaping on your lot also encourages surface infiltration of stormwater run-off, which is to be encouraged as an environmental sustainable outcome.
- 7.5 All lot areas within public view are to be landscaped within 3 months of the dwelling being completed and gardens and nature strips should be regularly maintained to create a pleasant landscape character for the estate.
- 7.6 The use of indigenous and native plant species is encouraged and a suggested plant species list is available from the Council. Any species recognised by Council to be an environmental weed will be prohibited from planting.
- 7.7 No pruning or removal of Council street trees is permitted without the prior written consent of the Responsible Authority.
- 7.8 Owners are encouraged to retain existing mature trees which are nominated on the Building Envelope Plan.
- 7.9 Letterboxes are to be integrated into any front fence, and house numbering must be clear and easily viewed from the street.
- 7.10 Tree planting in the rear yards of Lots 1-14 must be provided to form a canopy planting which will result in a visual buffer between the rear of the buildings and the neighbouring industrial premises.

8.1 All dwelling plans must achieve at least a 4 star rating by *First Rate* or an equivalent home energy rating scheme. Additionally, 5 star ratings will be encouraged.

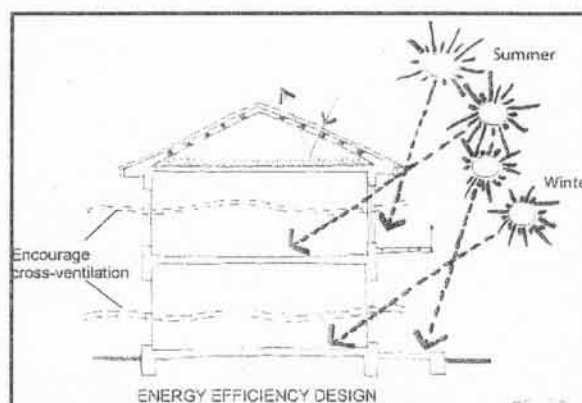
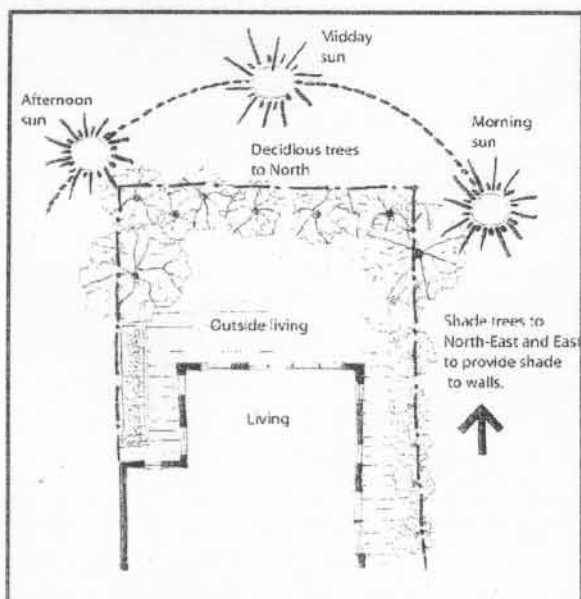
8.2 Dwellings should be sited and designed to achieve the following:

- oriented to make appropriate use of solar energy.
- sited and designed to ensure that the energy efficiency of existing dwellings on adjoining lots is not unreasonably reduced.
- designed with living areas and private open spaces located on the north side of the dwelling. For south facing lots, dwellings are to be designed so that solar access to north facing windows is maximised.
- internal room planning to maximise sunlight access through the dwelling.
- windows which are orientated for sun access in winter and cross-ventilation for summer.
- encourage landscape design that assists in facilitating entry of sunlight into the living areas of the dwelling, particularly in winter.

8.3 Sustainable Building Philosophy.

Elements that should be considered in the design of your house should include:

- Use of energy efficient appliances and gas appliances.
- Solar heating/hot water systems.
- Insulation in walls, ceilings and floors.
- Cross ventilation & appropriately located windows.
- Energy smart lighting.
- Thermal mass in building structures to control temperature.
- Collection and re-use of rainwater.
- Permeable surfaces in backyards.
- Incorporation of vegetation at high levels on roof gardens and walls, and use of deciduous trees to the north.



## WATER TANKS

9

- 9.1 Where possible the provision of non-reflective rainwater tanks is encouraged in the side and rear yards for garden use. Rainwater tanks must be screened from the street and cannot be located in the front yard.
- 9.2 Setback and build over easement requirements may exist which may be subject to building permits. Contact with an authorised building surveyor should be sought to determine whether special dispensation from the authority is required prior to a building permit being able to be issued.
- 9.3 The size and height of the rainwater tank must be appropriate to the open space and must not impact on the amenity of adjoining land by way of overshadowing, appearance and visual bulk. Further information can be obtained from City West Water's Conservation Programs Manager.



- 10.1 Where noted on the building envelope plan dwelling construction must conform to the noise attenuation techniques as specified in the Development Plan Overlay - Schedule 4.
- 10.2 The Council may, by a written consent, agree to the modification of the requirements set out in this clause with the exception of 10.3.
- 10.3 For all dwellings on lots 1- 15 on the Building Envelope Plan:
- The rear walls to form a continuous wall with adjoining buildings so that the height of the wall is not less than 5.5 metres above ground level.
  - No openable windows in a dwelling on the rear wall above ground floor level.
  - All Windows on the rear walls to be double glazed or constructed to a suitable design to the satisfaction of the responsible authority. Any habitable room for a dwelling, which adjoins the rear wall, will have a second aspect that provides external air to that room.
  - Shall be constructed in the following building materials:
    - > Wall (Double brick)
    - > Roof (Tiled roof or metal deck with 13mm plywood below)
    - > Ceiling (Double layer of 13mm plasterboard)
    - > Roof Insulation (100mm fibrous insulation 14kg/m<sup>3</sup>)
    - > Eaves (2 x 9mm cement sheet sealed to adjoining structure)
- 10.4 If the requirements of 10.3 are not to be complied with, a report will be provided by the owner, authorised by an acoustic engineer who is a member of the Institute of Engineers Australia. The report will demonstrate compliance with SEPP Noise N1 for the subject land and for all land capable of being used for a dwelling to the west of Lots 1 – 15.
- 10.5 For buildings on Lot 15 (possible mixed-use site) The following is required:
- > The design of the acoustic fence protecting all open areas adjoining the eastern boundary of the lot to be the same or greater standard than that required for lots 1-14.
  - > Buildings are protected from vibration caused by heavy vehicles.
  - > Dwelling occupants on floors above the ground floor are protected with provisions similar to or greater than those relating to lots 1-14 in the requirements of 10.3.
  - > Special attention is to be given to any east facing boundary wall including no habitable room openings or equivalent protection for windows that face east, and design of the structure to avoid vibration from heavy vehicle operations.



## COUNCIL ASSETS

11

- 11.1 The owner must notify Maribyrnong City Council in the event of any damage resulting to any Municipal assets, footpaths, kerb or channel, street pavement, street trees as a result of dwelling construction on their subject lot. The damaged assets must be reported to Council and reinstated to the satisfaction of the Responsible Authority at no cost to Council.
- 11.2 The street area and nature strip must be cleared of any waste building material, levelled and re-instated after completion of dwelling construction and construction of fences and gates.

## VARIATIONS

12

- 12.1 With the exception of the acoustic attenuation requirements for lots 1-15 the Maribyrnong City Council may consent to depart from the approved Design Guidelines upon written request (together with any plans) stating in detail the reason for the variation.

Maribyrnong City Council may approve the variation if:

- The variation is not detrimental to the streetscape or neighbourhood character of the area.
- The variation is designed to complement the dwelling design and siting.
- The variation will not cause an increase in detriment to any person.

A fee is payable to Maribyrnong City Council and this will be required to consider a request to give consent to depart from the Urban Design Guidelines.

## VERIFICATION

13

Prior to any works/construction of any building, the owner must provide Maribyrnong City Council (Urban Planning Office) with:

- A written verification/notice, prepared by the Building Surveyor responsible for issuing the Building Permit for the proposed dwelling on a lot, which states that the plans of the proposed Buildings on the lot comply with the Urban Design Guidelines and building envelopes on the endorsed subdivision plan.

A copy of the siting and development plans of the proposed dwelling to which the Building Surveyor's written statement relates.

16

# APPENDIX - BUILDING ENVELOPE PLAN

1



CATHCART ST.

VALNERE ST.

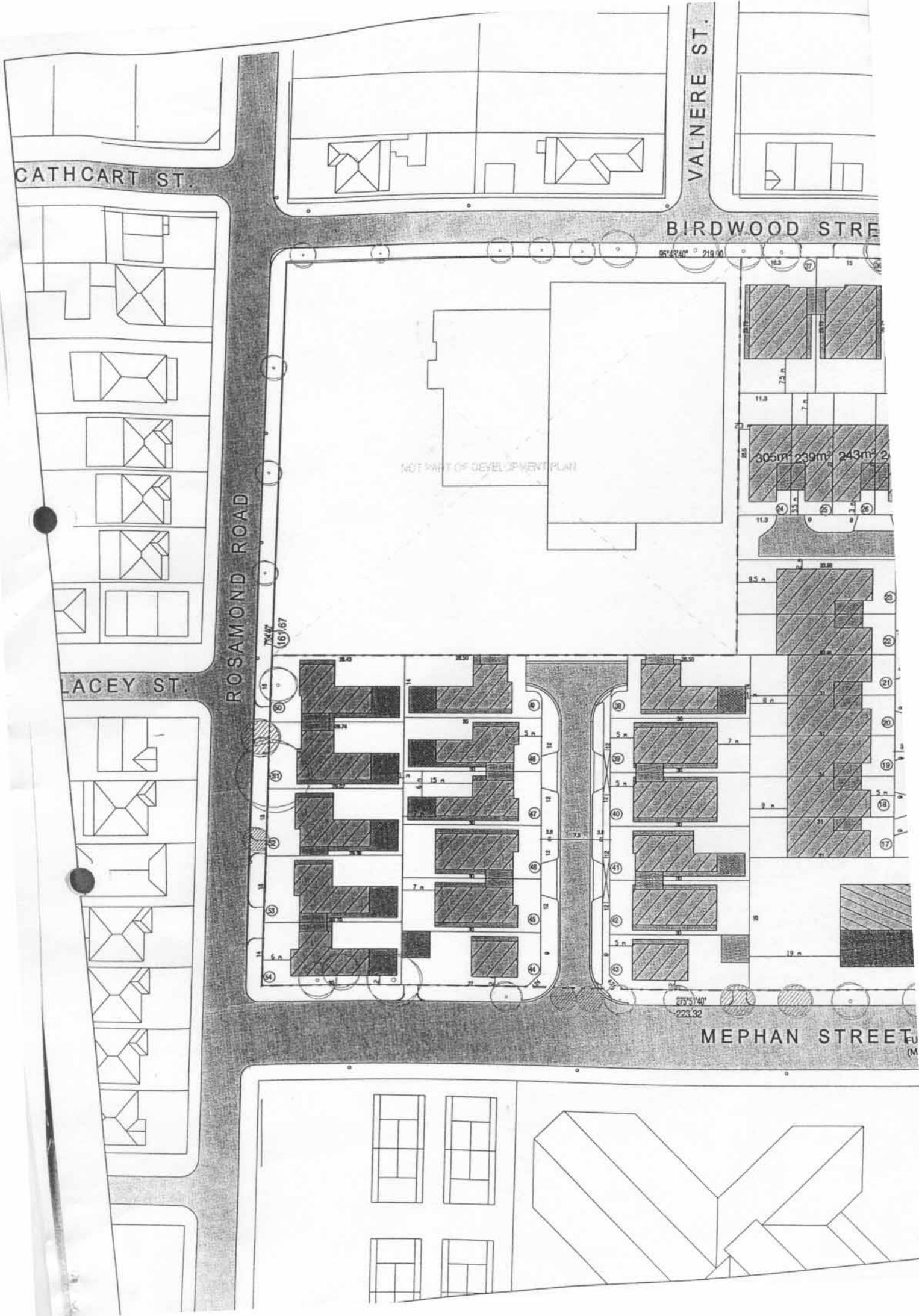
BIRDWOOD STREET

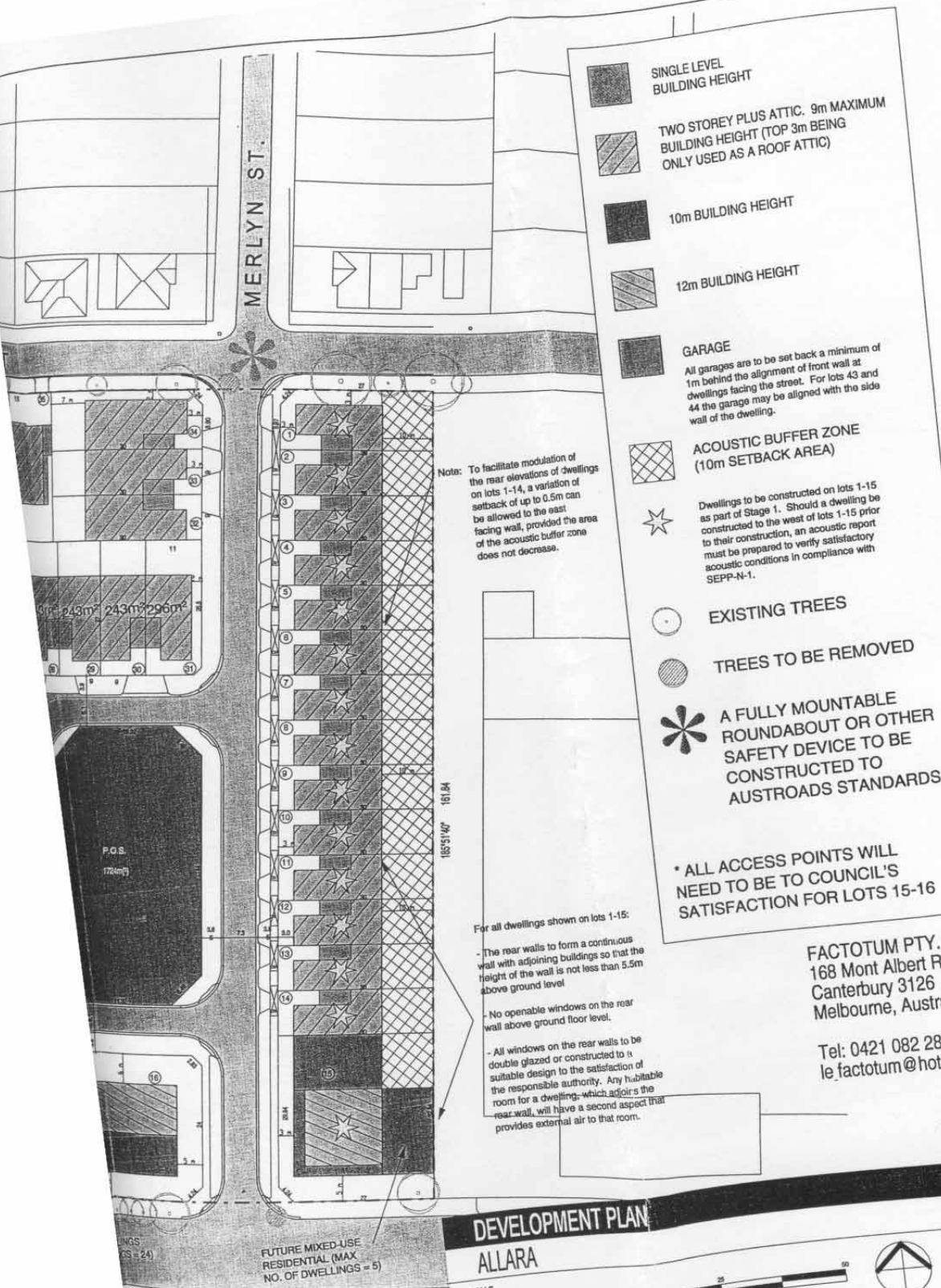
ROSAMOND ROAD

LACEY ST.

NOT PART OF DEVELOPMENT PLAN

MEPHAN STREET





FACTOTUM PTY. LTD.  
168 Mont Albert Road  
Canterbury 3126  
Melbourne, Australia

Tel: 0421 082 282 Fax: (03) 9436 1793  
le\_factotum@hotmail.com

ALLARA

FIGURE 4

CATHCART STREET

VALNERE STREET

BIRDWOOD STREET

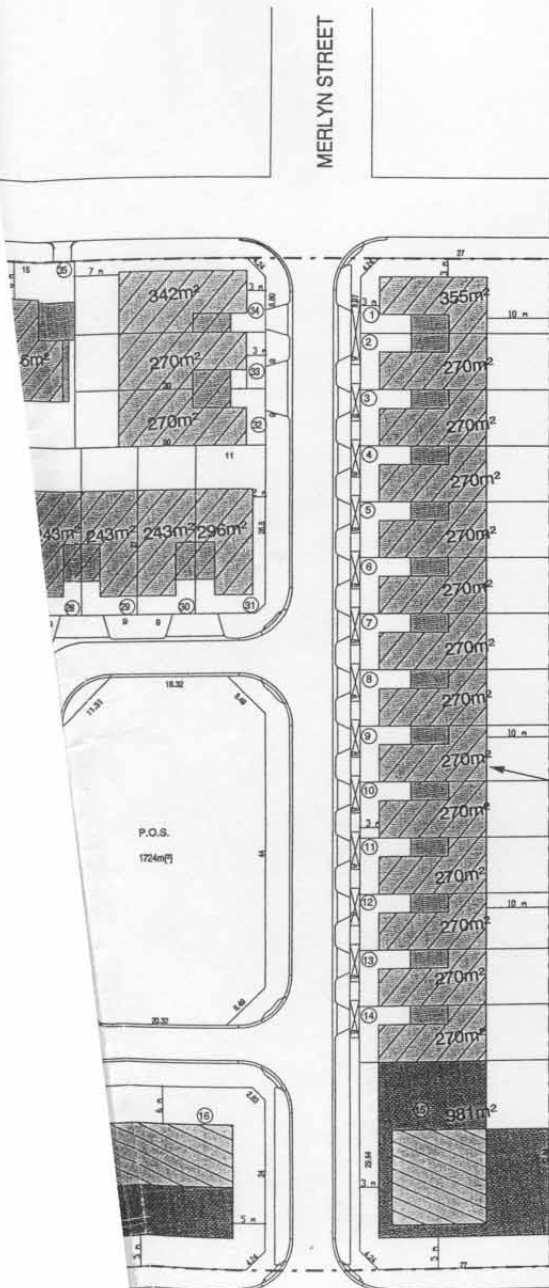
NOT PART OF DEVELOPMENT PLAN

ROSAMOND ROAD

MEPHAN STREET







MERLYN STREET

## LEGEND

- SINGLE LEVEL BUILDING HEIGHT
- TWO STOREY BUILDING HEIGHT
- 10m BUILDING HEIGHT
- 12m BUILDING HEIGHT
- GARAGE

Note: To facilitate modulation of the rear elevations of dwellings on lots 1-14, a variation of setback of up to 0.5m can be allowed to the east facing wall, provided the area of the acoustic buffer zone does not decrease.

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le.factotum@hotmail.com

ALLARA

FIGURE 5

## BUILDING ENVELOPE PLAN

ALLARA

SCALE

1:1000 @ A3

PROJECT NUMBER  
303066

DATE  
MAR 04



DRAWING NUMBER  
PD 01

CHECKED  
MJ

REVISION  
03

APPROVED  
BD



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